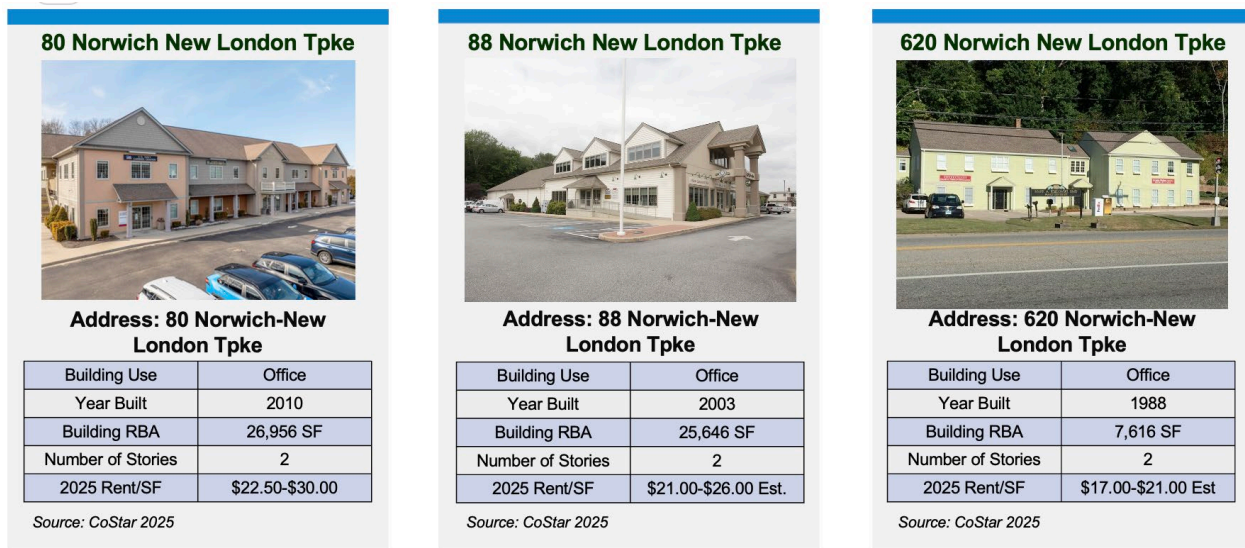


market fits this trend, with modest but steady demand from professional services, medical providers, and local institutions.

Office properties along Route 32, such as 80 Norwich-New London Turnpike (built in 2010, ~27,000 SF) and 88 Norwich-New London Turnpike (built in 2003, ~26,000 SF), have asking rents ranging from \$21 to \$30 per square foot, reflecting stable occupancy and moderate price appreciation. These properties generally offer two-story construction, ample parking, and proximity to retail and residential uses, making them attractive to small to mid-sized tenants.

Figure 25

Profiles of Recent Route 32 Office Developments



Development Pipeline: Limited Activity

Notably, there is **no new office development in the current regional pipeline**, either in Montville or the broader Study Area. This underscores the limited speculative interest in new office supply and reinforces the importance of maintaining and reinvesting in existing properties. Other commercial development activity in the pipeline—including retail, industrial, hospitality, and flex space—suggests that future land use demand may prioritize other asset classes over office.

Demand Outlook and Development Potential

Given Montville’s strong occupancy and rent growth, there may be **limited but targeted opportunities for new office development**, particularly if tailored to underserved or growing demand segments. These may include:

- **Medical office space**, especially given proximity to Backus Hospital and other healthcare anchors in Norwich and New London;
- **Professional services** catering to local government, legal, and financial industries;
- **Co-working and flexible office solutions** that serve remote workers and small businesses without requiring long-term lease commitments.

While Montville’s overall scale limits large-format office development, small infill projects (5,000–15,000 SF) or mixed-use developments with a modest office component could prove viable along Route 32, especially near key intersections or adjacent to new residential projects.

Constraints to Office Development

Several constraints should be considered:

- **Lack of speculative investment:** Lenders remain hesitant to finance new office construction in tertiary markets unless backed by a strong anchor tenant.
- **Zoning and infrastructure:** Existing zoning may require modification to support new formats or mixed-use development. Additionally, utility and broadband infrastructure must be evaluated to support modern office needs.
- **Tenant preferences:** Demand is increasingly driven by user preferences for amenity-rich, flexible, and sustainable spaces—features that may require retrofitting older office stock or thoughtful design in new projects.

Town of Montville

Technical Assistance Grant Application

Connecticut Department of Housing

Executive Summary

The Town of Montville respectfully requests \$125,000 in Technical Assistance funding from the Connecticut Department of Housing to support the housing-related components of a comprehensive land-use framework update. This work will be highly informed by the Town's ongoing CIF Round 5–funded **Revive & Renew Routes 32 and 163** study (www.renew163-32.com), which is identifying redevelopment sites and future housing opportunities along these key corridors. Building directly on this corridor work, the requested funding will enable Montville to modernize its Plan of Conservation and Development (POCD), adopt housing-supportive zoning reforms including adaptive reuse regulations, update subdivision standards, and develop objective design guidelines that comply with HB 8002 requirements.

With a total project cost of \$330,000, the Planning and Zoning Commission will be requesting a item of \$205,000 for FY 2027. The requested \$125,000 will fund specific housing-focused tasks within a broader Comprehensive Town Plan update, including development of a renewed Housing Plan, implementation of Transit-Oriented Development and Housing Growth Zones, parking reforms, streamlined approval processes, and design standards that facilitate rather than obstruct housing production.

Montville is uniquely positioned to undertake this work. The Town has a demonstrated commitment to housing across income levels, already limits unnecessary special permits for residential development, and is building momentum through an ongoing CIF Round 5–funded Revive & Renew Routes 32 and 163 study. With the recent passage of HB 8002 and community engagement underway, this is the optimal moment to translate housing vision into implementable regulations and policies.

DOH-funded tasks will commence immediately upon grant award and will be fully integrated into the broader planning effort. Even if local funds are partially allocated, the Town will proceed using a phased approach that prioritizes housing policy, regulatory modernization, and implementation measures aligned with state housing objectives.

Statement of Need

Current Planning Framework

Montville's existing land-use framework is outdated and no longer reflects current housing needs, statutory requirements, or market conditions. The Town's POCD was adopted in 2010 and received only a minimal administrative update in 2022 to remain technically "current" under state law as land use staff transitions were occurring. However, this document has never been comprehensively vetted through a robust public outreach process, nor does it incorporate recent demographic trends, housing demand projections, or changes in state housing policy.

As a result, the existing POCD does not adequately guide housing growth, redevelopment opportunities, or infrastructure investment. It cannot serve as an effective foundation for zoning decisions, capital planning, or housing strategy because it does not capture the full housing potential or long-term vision for the community. The Town leadership recognizes this challenge and will endeavor to provide the necessary funding for fiscal year 2027.

Similarly, Montville's zoning and subdivision regulations have not been comprehensively updated in many years. While the Town has historically embraced housing development and has limited the use of special permits for both residential and commercial projects, the current regulatory framework lacks modern tools to proactively support housing production, streamline approvals, and implement the Transit-Oriented Development (TOD) and Housing Growth Zones now authorized under state law.

The Housing Challenge

Like many Connecticut communities, Montville faces growing pressure to accommodate housing demand while maintaining fiscal sustainability and community character. The Town recognizes that outdated regulations create unnecessary barriers to housing development, delay project approvals, and fail to provide clear guidance for developers and residents alike. With the housing demands of southeastern Connecticut driven by the defense industry and Electric Boat's hiring, Montville can assist in providing housing opportunities while establishing frameworks to create a destination for families.

Without updated planning documents and regulations, Montville cannot:

- Effectively implement HB 8002 requirements
- Leverage state housing funding and incentive programs
- Guide housing development to appropriate locations
- Establish clear, objective standards for residential projects
- Support mixed-use redevelopment in key corridors
- Address workforce and affordable housing needs strategically

Building on Existing Momentum

The need for a coordinated planning update is further reinforced by the Town's ongoing CIF Round 5–funded **Revive & Renew Routes 32 and 163** study. This corridor planning effort has generated significant community momentum around mixed-use redevelopment, walkable environments, and housing opportunity areas along Montville's primary commercial corridors.

With that work underway and HB 8002 requirements now in effect, Montville is at a critical juncture where a community-engaged Comprehensive Town Plan update can translate corridor vision into housing-ready regulations and actionable implementation strategies. This Technical Assistance request represents the natural next step: taking community input and corridor planning concepts and embedding them into the Town's foundational planning and regulatory documents.

Montville's Commitment to Housing

Montville has a strong track record of welcoming housing across income levels and supporting families throughout the community. The Town already minimizes reliance on special permits for housing development, reducing regulatory uncertainty and streamlining approvals. This pro-housing approach positions Montville to move quickly from planning to implementation once updated regulations are in place.

The Town is committed to adopting new tools that proactively address future housing demand, including:

- Transit-Oriented Development districts leveraging SEAT bus service
- Housing Growth Zones in appropriate locations
- Reduced parking minimums that lower development costs
- Administrative and summary review procedures
- Fair Rent policies integrated into land-use administration

Technical Assistance funding will allow Montville to move from intent to execution by developing modern, legally compliant, and housing-ready plans and regulations that support these objectives.

Project Goals and Objectives

This Technical Assistance project will achieve the following housing-focused goals:

Goal 1: Establish a Housing-Ready Policy Framework

Develop a comprehensive, updated POCD that identifies housing opportunity areas, establishes production targets, and aligns land use, transportation, and infrastructure policy with housing

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growth objectives. Create a Housing Plan in conformance with HB 8002 timelines which will be integrated by reference into the POCD.

Objectives:

- Create a renewed Housing Plan that meets HB 8002 recommendations
- Identify suitable locations for housing development and redevelopment
- Establish targets for total and affordable housing unit production
- Integrate Fair Rent principles into municipal land-use administration

Goal 2: Modernize Zoning Regulations to Support Housing Production

Reform zoning regulations to remove unnecessary barriers, reduce discretionary review, and implement state-authorized housing tools.

Objectives:

- Reform parking minimums to reduce development costs
- Expand administrative and summary review procedures
- Evaluate and adopt Transit-Oriented Development districts
- Evaluate and adopt Housing Growth Zones
- Minimize reliance on special permits for residential development
- Create clear, use-specific standards that provide regulatory certainty

Goal 3: Update Subdivision Standards

Modernize subdivision regulations to support efficient residential development, diverse housing types, and cost-effective land use.

Objectives:

- Incorporate modern planning and engineering best practices
- Ensure consistency with current State Statutes
- Remove obstacles to housing-supportive subdivision design
- Support a range of residential development types

Goal 4: Develop Objective, Housing-Oriented Design Guidelines

Create clear, objective design standards that facilitate housing development while complying with HB 8002 limitations on subjective "community character" requirements.

Objectives:

- Establish predictable standards for multifamily housing
- Support housing density appropriate to context
- Reduce discretionary review and approval delays

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- Provide easily interpreted guidance for developers and staff
- Align design expectations with POCD housing goals

Goal 5: Build Community Consensus and Implementation Capacity

Ensure meaningful public engagement and develop the organizational capacity necessary to implement new housing policies and regulations.

Objectives:

- Conduct public outreach through multiple channels
- Establish a Steering Committee to guide plan development
- Build staff capacity to administer new regulations
- Create clear implementation timelines and milestones
- Position Montville to leverage future state housing funding

Scope of Work: Housing-Focused Components

The Town proposes to undertake a Comprehensive Town Plan update with total project costs of \$320,000. This Technical Assistance request focuses specifically on the housing-related components, totaling \$125,000.

Task 1: Plan of Conservation and Development (POCD) and Housing Plan

Total Task Cost: \$175,000

Housing Plan Component Requested: \$75,000

Timeline: 12 months (Housing Plan Adopted June 2027)

Central to this effort will be development of a renewed Housing Plan that meets the intent and recommendations of HB 8002 and supports adoption of a Fair Rent policy.

The updated POCD will function as Montville's primary housing and growth roadmap, providing clear direction for future land-use decisions, infrastructure investment, and regulatory modernization. This document will identify suitable areas for housing development, establish targets for total and affordable housing production, and align land use, transportation, economic development, and infrastructure policy with housing goals.

Housing-Specific Elements:

Housing Opportunity Areas and Redevelopment Sites

The Housing Plan will identify specific geographic areas suitable for housing growth, including:

- Utilize the State's housing unit goals specific to Montville (expected December 2026)

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- Underutilized commercial corridors along Routes 32 and 163; expand upon the Revive and renew visions to be completed in April 2026. This Housing plan will bring the visions further along in vision and incremental development/implementation steps;
- Evaluate TIF as an option to support housing infrastructure, in addition to other state grant programs established under HB 8002.
- Evaluate water and sewer needs and water/sewer extensions and upgrades.
- Analyze town wide landscape developable land and marry to existing infrastructure
- Identify key ‘greenfield, greyfield and redevelopment/infill sites’
- Transit-accessible locations near SEAT bus routes

Transit-Oriented Development (TOD) Districts

Montville qualifies for TOD designation under HB 8002 due to SEAT bus service that meets frequency and coverage requirements. The Housing Plan will evaluate potential TOD districts and establish parameters for their implementation, including:

- Appropriate locations based on transit access
- Density and use standards
- Infrastructure considerations
- Integration with corridor planning efforts

Housing Growth Zones

The Plan will evaluate opportunities for Housing Growth Zones as authorized by HB 8002, identifying locations where streamlined approvals and reduced regulatory requirements can facilitate housing production.

Housing Production Targets

Working with regional data, demographic projections, and market analysis, the Housing Plan will establish realistic targets for:

- Total housing unit production over 5- and 10-year periods
- Affordable and workforce housing development
- Mix of housing types (single-family, multifamily, mixed-use)
- Alignment with regional housing needs

Fair Rent Policy Integration

The project will evaluate and consider the best approach to fair rent policies. This includes considerations for a local commission v. joining a regional approach.

Community Engagement Process

To ensure public buy-in and reflect local knowledge, this task includes extensive community engagement:

Steering Committee

A Steering Committee will be established to take an active role in plan development. This committee will include representation from:

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- Planning and Zoning Commission
- Other relevant boards and commissions
- Business community
- Housing advocates
- Residents from diverse neighborhoods

Public Outreach Sessions

Multiple public workshops will be held at various stages of plan development to:

- Gather input on housing needs and opportunities
- Review draft recommendations
- Build consensus around implementation strategies
- Address community concerns

Surveys and Digital Engagement

Online surveys and a dedicated project website will provide additional opportunities for input, ensuring broad participation across demographic groups and geographic areas.

Housing Plan Cost Breakdown

Item #	Task	Cost
1	Initiation, Mapping and Data Collection	\$ 5,000
2	Analysis of Existing Conditions	\$ 5,000
3	Evaluate existing water/sewer service areas and capacity, including necessary calculations	\$ 15,000
4	Conduct Developable Land Analysis/Verify State level mapping to be completed by State	\$ 5,000
5	Identify Sites: conduct site level analysis and concepts; includes Greenfields, greyfield, infill	\$ 12,500
6	Advance Revive and renew Vision specific to housing	\$ 10,000
7	Community Engagement	\$ 10,000
8	Report and Meetings	\$ 10,000
9	Expenses	\$ 2,500
	Total	\$ 75,000

Deliverables:

- Comprehensive rewritten POCD incorporating housing throughout all elements
- Standalone Housing Plan chapter meeting HB 8002 recommendations

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- Housing opportunity area maps
- Housing production targets and implementation timeline
- Fair Rent policy draft ordinance
- Community engagement summary and findings report

Task 2: Zoning Regulations Update (Housing-Focused)

Total Task Cost: \$100,000

Housing-Specific Component Requested: \$30,000

Timeline: 15–18 months

The housing-focused portion of this task will directly address HB 8002 requirements and implement recommendations from the Housing Plan.

Housing-Specific Regulatory Reforms:

Parking Minimum Reforms

Current parking requirements often increase housing development costs unnecessarily. This task will:

- Review existing parking minimums for all residential uses
- Implement HB 8002 parking reforms
- Establish reduced or eliminated parking minimums near transit
- Create shared parking provisions for mixed-use development
- Allow flexibility for adaptive reuse projects

Expanded Administrative and Summary Review

To streamline housing approvals, updated regulations will:

- Expand the range of housing projects eligible for staff-level review
- Establish clear criteria for summary review procedures
- Reduce reliance on discretionary special permits
- Create predictable timelines for project approvals

Transit-Oriented Development (TOD) Districts

Building on POCD recommendations, this task will:

- Establish TOD zoning district standards
- Define appropriate density, height, and use parameters
- Create incentives for mixed-use development with residential components
- Ensure standards align with transit service characteristics
- Integrate TOD provisions with corridor design guidelines

Housing Growth Zones

The regulations will establish Housing Growth Zones as authorized by HB 8002:

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- Identify appropriate locations based on infrastructure capacity
- Establish streamlined approval processes
- Define density bonuses and other incentives
- Ensure consistency with POCD housing targets
- Create clear standards that provide regulatory certainty

Special Permit Reduction

Montville already limits special permit requirements, but this task will further reduce unnecessary discretionary review by:

- Converting appropriate special permit uses to permitted or site plan review
- Establishing objective approval criteria where special permits remain necessary
- Creating clear timelines and decision standards
- Minimizing opportunities for arbitrary denial

Use-Specific Standards

Updated regulations will provide clear, objective standards for various housing types:

- Single-family residential development
- Two-family and duplex housing
- Multifamily housing
- Mixed-use residential development
- Accessory dwelling units
- Senior housing

Deliverables:

- Entirely new comprehensive zoning regulations with housing-focused provisions
- TOD district standards and maps
- Housing Growth Zone regulations and maps
- Parking reform provisions
- Administrative and summary review procedures
- Use-specific standards for all residential development types
- Conversion table showing changes from existing regulations

Task 3: Subdivision Regulations (Housing-Related Update)

Total Task Cost: \$30,000

Housing-Specific Component Requested: \$5,000

Timeline: 6 months

The project will update Montville's subdivision regulations to reflect modern planning, legal, and engineering standards and ensure consistency with current State Statutes. While subdivision regulations apply to all development, this housing-focused component will ensure that standards support diverse housing types, efficient land use, and cost-effective residential development.

Housing-Related Elements:

Flexible Design Standards

Updated subdivision regulations will accommodate diverse housing types:

- Appropriate standards for various lot sizes and configurations
- Flexibility for cluster development and open space preservation
- Standards that support multifamily and mixed-use subdivisions
- Provisions for innovative housing arrangements

Infrastructure Efficiency

Regulations will encourage cost-effective infrastructure design:

- Right-sizing of road widths and parking areas
- Efficient utility placement
- Reduced site development costs where appropriate
- Standards that support housing affordability

Coordination with Zoning

Subdivision standards will align with housing-supportive zoning provisions:

- Consistency with TOD and Housing Growth Zone objectives
- Support for POCD-identified housing opportunity areas
- Clear relationship between subdivision and zoning requirements

Deliverables:

- Updated subdivision regulations incorporating modern standards
- Housing-supportive design provisions
- Consistency review with zoning and POCD
- Implementation guidance for staff

Task 4: Design Guidelines

Total Task Cost: \$25,000

Housing-Specific Component Requested: \$15,000

Timeline: 15–18 months (coordinated with zoning updates)

To comply with HB 8002's limitations on subjective "community character" standards, Montville will develop clear, objective design guidelines for multifamily housing and mixed-use corridors along Route 32 and portions of Route 163. These guidelines will be developed in tandem with zoning regulation updates to ensure consistency and ease of implementation.

Objective Design Standards:

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Multifamily Housing Guidelines

Clear, measurable standards will address:

- Building massing and articulation
- Façade design and material requirements
- Parking and vehicular access design
- Pedestrian connectivity and site design
- Landscaping and screening
- Relationship to street and adjacent uses

Mixed-Use Corridor Standards

Building on the Routes 32 and 163 corridor study, guidelines will establish:

- Ground-floor activation standards for mixed-use buildings
- Upper-story residential design requirements
- Parking placement and screening
- Streetscape integration
- Signage and lighting standards

Compliance with HB 8002

All design guidelines will:

- Use objective, measurable criteria
- Avoid subjective "community character" language
- Provide clear approval standards
- Minimize discretionary review
- Support housing production goals

Ease of Administration

Guidelines will be developed for practical implementation:

- Clear illustrations and examples
- Checklists for staff and applicant review
- Streamlined approval processes
- Integration with zoning standards

Deliverables:

- Comprehensive design guidelines document
 - Multifamily housing design standards
 - Mixed-use corridor design standards
 - Illustrative examples and diagrams
 - Staff implementation guide
 - Checklist tools for application review
-

Project Schedule

The housing components of the project will commence immediately upon grant award. This assumes a March/April 2026 initiation. The first 2-3 months will entail data collection, mapping and stakeholder meetings. At this time, July 1 FY 2027 budget will take effect. Should the town funds be allocated as noted above, the full project will commence and coinciding with the baseline information gathered during the first months. In any event, it is envisioned the housing plan and all related housing regulatory matters will be completed by June 2027. With full funding, the POCD and comprehensive regulatory updates will be completed by March 2028.

Budget

Total Project Cost: \$320,000

Task	Total Cost	Housing-Specific/DOH Request	Planned FY 2027 Budget Request
Task 1: POCD & Housing Plan	\$175,000	\$75,000	\$100,000
Task 2: Zoning Regulations	\$100,000	\$30,000	\$70,000
Task 3: Subdivision Regulations	\$30,000	\$5,000	\$25,000
Task 4: Design Guidelines	\$25,000	\$15,000	\$10,000
Legal Review	\$20,000		
TOTAL	\$350,000	\$125,000	\$205,000

Local Commitment

Community Engagement Plan

Meaningful community engagement is essential to building consensus around housing policy. Montville's engagement strategy includes multiple touchpoints to reach diverse audiences.

Advisory Committee: A Town Council-established advisory committee will guide plan development, meeting monthly with representatives from Planning and Zoning Commission,

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Economic Development Commission, Public Safety Committee, Public Works, WPCA, Conservation Commission, IWWC, Town Council, Board of Finance, business community, residents, housing advocates, and youth/young families.

Public Workshops: Multiple workshops will be held at key milestones throughout the planning process, with topics and timing determined based on project needs and community input.

Online Engagement: A dedicated project website will provide project background, draft documents, surveys, meeting announcements, FAQs, and contact information.

Focused Stakeholder Meetings: Additional meetings will be held with developers, real estate professionals, housing advocates, business owners, adjacent municipalities, SECCOG, and state agencies.

Accessibility: All activities will include multiple meeting times, virtual participation options, translation services if needed, plain language materials, and outreach to underrepresented communities.

Alignment with State Housing Priorities

This project directly advances Connecticut's housing goals and implements key provisions of recent state housing legislation.

HB 8002 Implementation

The project comprehensively addresses HB 8002 requirements:

Housing Plan Development

While HB 8002 made Affordable Housing Plans optional, it encourages Housing Growth Plans. Montville's renewed Housing Plan will incorporate Housing Growth Plan elements, including identification of suitable housing areas and establishment of production targets.

Transit-Oriented Development Districts

Montville qualifies for TOD designation based on SEAT bus service. The project will evaluate and establish TOD districts, taking advantage of streamlined approval processes and state incentives available to communities adopting these tools.

Housing Growth Zones

The project will identify appropriate locations for Housing Growth Zones, establishing the regulatory framework necessary to leverage state resources and funding programs tied to these designations.

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Parking Reforms

Updated zoning regulations will implement parking minimum reforms required by HB 8002, reducing development costs and supporting housing affordability.

Summary Review Procedures

Expanded administrative and summary review processes will streamline housing approvals, reducing timelines and regulatory uncertainty.

Objective Design Standards

Housing-oriented design guidelines will comply with HB 8002 limitations on subjective "community character" standards, providing clear, objective criteria that facilitate rather than obstruct housing development.

Fair Rent Policy

The project includes development and integration of a Fair Rent policy into municipal land-use administration, supporting housing stability and tenant protections.

Regional Coordination

All planning efforts will be coordinated with the Southeastern Connecticut Council of Governments (SECCOG) Regional Plan of Conservation and Development, ensuring consistency with regional housing goals and supporting interjurisdictional cooperation.

State Plan of Conservation and Development

Project goals and recommendations will align with the State Plan of Conservation and Development's housing objectives, including:

- Promoting diverse housing options
- Supporting transit-oriented development
- Removing regulatory barriers
- Advancing housing affordability
- Encouraging sustainable growth patterns

Conclusion

The Town of Montville requests Technical Assistance funding to build the regulatory and policy foundation necessary to deliver housing outcomes, implement HB 8002, and position the community to leverage future state and federal housing investments.

This project represents a critical opportunity to modernize an outdated planning framework, remove barriers to housing production, and create clear pathways for diverse housing

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development. With strong local commitment, experienced consultant support, ongoing corridor planning momentum, and a demonstrated track record of welcoming housing, Montville is prepared to translate this funding into meaningful housing policy reform and implementation.

The requested \$125,000 investment will yield:

- A comprehensive, housing-ready POCD and Housing Plan
- Modern, HB 8002-compliant zoning regulations
- Streamlined subdivision standards supporting residential development
- Objective design guidelines facilitating housing production
- Implementation capacity to achieve housing goals

Most importantly, this funding will enable Montville to move from intent to action, transforming housing policy into housing outcomes that serve current residents and welcome future community members across all income levels.

We respectfully request your favorable consideration of this application and look forward to partnering with the Connecticut Department of Housing to advance housing opportunity in Montville and throughout the region.

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The following are new definitions to be incorporated alphabetically.

1.3 DEFINITIONS

Architectural Façade Element. A permanent, three-dimensional component or treatment applied to, integrated with, or projecting from, or recessed within a building's facade that contributes to the visual articulation, scale, rhythm, or architectural character of the building facade. Architectural facade elements may include, but are not limited to, windows and window groupings, doors, porches, stoops, balconies, bay windows, columns, pilasters, canopies, awnings, arcades, reveals, projections, recesses, changes in plane, and decorative or functional architectural detailing. Architectural facade elements do not include applied graphics, murals, signage, or temporary features unless expressly permitted elsewhere in these Regulations.

Blank Wall (Building and Structure). A segment of a building's or structure's wall of 250 SF or greater which contains no Architectural Façade Element.

Building Line. A line drawn along the building facade facing the street and extending to each side property line.

Decision-Making Authority. The Planning and Zoning Commission or Director of Land Use and Development. **Reconcile throughout zoning regulations.**

Director. Means the Director of Land Use and Development, as the title may be amended from time to time.

Fenestrations. The arrangement, placement, size, proportion, spacing, and design of windows, doors, and other openings in a building facade, including the relationship of such openings to one another and to the overall architectural composition of the building. Fenestration includes glazing, framing, and operable or fixed elements that admit light, air, or access, but does not include solid wall areas without openings.

Site Appurtenances. Structures, equipment, facilities, or installations that are necessary for the provision of public or private utility services for a principal or accessory use operation, maintenance, safety, or service. Site appurtenances may include, but are not limited to, poles, wires, conduits, transformers, meters, valves, hydrants, manholes, handholes, vaults, cabinets, switchgear, junction boxes, guy wires, solar panels and arrays, anchors, ground mounted or raised pads, HVAC equipment, control equipment, generators and similar devices. Site Appurtenances may be located above or below ground, upon roof tops or wall mounted upon buildings or structures. These systems generally include but are not limited to:

1. Trash and Recycling Enclosures
2. Roof Mounted Equipment
3. Wall-Mounted Equipment and Service Lines
4. Ground-Mounted Utility Equipment
5. Generators
6. Transformers
7. Fuel Storage Systems

Town. The Town of Montville, general government.

Parking Related Terms

Parking Stall. A space intended for the temporary parking of a motor vehicle that serves visitors or employees of a principal use.

Access Drive. A drive providing access into, out of and through a site and does not accommodate Parking Stalls. **Reconcile this and Drive Aisles through out zoning regulations.**

Drive Aisle. A vehicular travel-way which provides direct access to Parking Stalls.

Parking Area. The area between all surface areas which accommodate parking stalls and all drives, islands and aisles serving parking that serves the use(s) located on or off the property.

Parking Row. The sequence of Parking Stalls running parallel to the Drive Aisle

Landscape Related Terms

Shade Tree. A deciduous tree which at maturity achieves a canopy width of 25' or greater and height of 30' or greater and offers dense leaf coverage. Intended to reduce heat gain.

Groundcover Vegetation. For the purpose of these regulations, includes evergreen plantings no higher than eighteen inches at maturity and is planted with spacing to create a continuous mat of ground coverage. Groundcover Vegetation may be one or more plant species planted in a uniform manner. Lawn is not considered ground cover.

Desire Line. A pedestrian's preferred direction of travel between two points.

Street Tree. A deciduous tree which has high or upright branching habit to achieve safe sight lines and overhead clearance for pedestrians and vehicles; generally, provides dense leaf coverage for shade and are generally of medium to large size depending upon application.

Architectural Vegetation. A tree or shrub whose natural growing habit is uniform in shape and utilized to accent, complement or reinforce building elements or guide a person's movement or sightline.

Non-Invasive Native/Adaptive Plant Material. Trees, shrubs and groundcover, including cultivars, which are native to, and have demonstrative adaptability to, the climate of the USDA Hardiness Zone and which are not Invasive Plants.

Invasive Plants. Plant material listed on the CT Invasive Plant List as maintained by the CT Invasive Plant Council.

Landscape(d). An area of permeable surface containing vegetation such as turf, shrubs, perennials and/or annual plantings. Landscaped areas do not include solely stone or natural mulched materials.

Landscaped End Island. A raised curbed Landscaped area located at the end of a Parking Row.

Landscaped Intermediate Island. A raised curbed Landscaped area located within a Parking Row.

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Landscaped Median Island. A Landscaped area which Parking Stalls are perpendicular to and are raised by curbing when not used for stormwater/LID measures.

Partial Visual Landscape Buffer. TBD

Complete Visual Landscape Buffer. TBD

Lighting Related Terms

FOOTCANDLE: A unit of illumination equivalent to the illumination produced by one candle at a distance of one foot and equal to one lumen per square foot

Luminaire

Light Pole

BUG.

Light Base.

Glare: The visual impact of a light emission resulting from insufficient shielding of a source of light which may create hazards or discomfort to any person off of the property. Glare may be created directly from a light fixture

Light Trespass

Security Lighting

Nadir. The point on the ground perpendicular to the light source above.

Photometric Plan.

Full cut-off.

Use related Terms

Personal Service Establishment. An establishment which offers specialized goods and services to the consumer such as barbers, beauty salons, laundry, cleaning, tailoring, massage, clothing repair, real estate, legal services and tax/accounting services.

Mixed Use Development. A development containing one or more multifamily or single-family dwelling units and one or more commercial, public, institutional, retail, office or industrial uses (See CGS 8-13m).

Retail. A commercial establishment that provides goods directly to the consumer, where such goods are available for immediate purchase and removal from the premises by the purchaser.

Retail, Large. A single Retail establishment 25,000 SF or larger.

Retail, Small. A single Retail establishment less than 25,000 SF.

Additions to:

SECTION 17

17.2 DECISION MAKING AUTHORITY

The Commission hereby delegates to the Director, or his/her designee, Decision-Making Authority of Site Plan Applications submitted pursuant to Section 17 of these Regulations except for the following applications:

- a. A Site Plan Application in conjunction with a Special Permit;
- b. A deviation is requested as set forth in Section 17.3. Deviations

The Director shall have the sole discretion to refer any application to the full Commission, which would then become the decision-making body.

17.3 DEVIATIONS

1. To provide flexibility that will promote rehabilitation of existing buildings, the use of lots created prior to the town adopting Zoning or Subdivision Regulations, and new construction, the Commission may approve an application that deviates from the minimum or maximum requirements of these regulations by up to 15 percent in industrial, commercial, multi-family and mixed-use zones if the Commission finds that:
 - a. The adjustment from the minimum or maximum requirements is consistent with all relevant purpose and intent statements of these zoning regulations and with the general purpose and intent of the plan of conservation and development; and
 - b. The adjustment will not have a substantial or undue adverse effect upon adjacent property, the character of the area, or the public health, safety, and general welfare.
2. Deviations shall not be applied to improvements after they have been constructed.
3. The provisions of this section shall not be construed to allow the Applicant or Applicant's agents/contractor the independent flexibility during construction.
4. For the purpose of this regulation, 15% shall be calculated using the following guideline example:

If the requirement to be deviated is based upon a total area, the deviation shall be based upon initial requirement. For example, a 10% building coverage of a 40,000 SF lot shall not exceed 4,000 SF. This requirement may be deviated by 15% of the 4,000; thus allowing 4,600 SF of coverage.
5. Deviations may not be applied to the following:
 - a. Standards along a property line abutting a residential district;

---DRAFT---

- b. Standards which will create a non-conforming lot;
- c. Minimum open space/recreation requirements;
- d. Landscape requirements

Renumber sections: 17.2 Procedure becomes 17.4, etc.

SECTION 18 OFF-STREET PARKING REGULATIONS

18.1 GENERAL

No building or structure shall be erected, enlarged, modified, or its use changed unless permanently maintained parking and loading spaces are provided in accordance with the provisions of these Regulations. In addition, no alterations, improvements or modifications to an existing parking lot or the establishment of a new parking area may be established until a site plan in accordance with Section 17 of these Regulations has been approved by the Decision-Making Authority .

An existing parking area and appurtenances legally existing prior to the establishment of these Regulations or amendments thereto, which does not comply with the requirements of this section shall be considered a non-conforming use of land.

The Decision-Making Authority shall review the location of access drives across a public right-of-way proposed as part of a site plan. The applicant shall be responsible for obtaining an encroachment review from the DPW and/or State of CT DOT. Applicant shall be responsible for obtaining a traffic review from the State of CT Office of State Traffic Authority (OSTA) as required.

It is the intent of these Regulations to allow flexibility in addressing vehicle parking, loading areas, parking lot landscape design, storm water management and to promote transit orientated development.

18.2 PARKING SPACE DIMENSIONS

18.2.1 A parking space shall have a minimum rectangular dimension sufficient to provide one hundred eighty square feet (180sqft) of parking area exclusive of driveways and shall be permanently reserved for the temporary parking of one (1) automobile. The length of any parking space shall be at least eighteen feet (18') and the width at least nine feet (9') as marked on an axis parallel with the vehicle after it is parked. The dimension of the parking spaces shall be consistent for all contiguous spaces and shall provide for the most efficient design for vehicular and pedestrian circulation.

18.2.2 The Decision-Making Authority shall accept parking plans; if one or more of the following criteria have been met:

1. A parking plan has been submitted by a Connecticut Licensed Professional Engineer which has been reviewed and approved by the Town Engineer and Director of Planning.
2. The construction of fewer parking spaces may improve stormwater quality while still satisfying parking demand.
3. Current parking demand will allow for the phasing of required parking spaces. The construction of the parking area and installation of the spaces

may be phased according to the short term requirements as designated on the Plan, except that no less than fifty percent (50%) of the total spaces required shall be constructed as part of the short or current requirement. The balance of spaces not constructed shall be designated as reserve spaces on the site plan and laid out on an integral part of the overall parking layout, and must be located on land suitable for parking area development and either left in its natural state or suitably landscaped. Under any circumstances, the owner may construct the total number of parking spaces required or if the Decision-Making Authority determines that additional spaces identified as reserve spaces on the site plan may be required, the Decision-Making Authority shall notify the property owner concerning the Decision-Making Authority's findings and owner shall construct the required spaces within six (6) months of such notification.

18.3 PARKING REQUIREMENTS

A Parking plan may be submitted by a Connecticut Licensed Professional engineer which has been reviewed and approved by the Town Engineer and the Director of Planning in lieu of parking requirements listed in Section 18.3.

Adequate parking facilities located off the street or highway right-of-way shall be provided to accommodate the motor vehicles of all occupants, employees, customers, and other persons normally visiting each building or premises at any one time in accordance with the following requirements:

- 18.3.1 For One-Family or Two-Family Dwelling Units: Two (2) spaces for each family unit; driveways may be included as a required space for single-family dwellings. For permitted home occupations, spaces shall be provided as required for the occupational uses specified herein:
- 18.3.2 Sixteen (16) or fewer Dwelling Units. There shall be no parking minimum.
- 18.3.3 Greater than sixteen (16) Dwelling Units.
 - a. Studio and one-bedroom Dwelling Units: 1 parking space per Dwelling Unit
 - b. Two or more-bedrooms Dwelling Units: 2 parking spaces per Dwelling Unit
 - c. Fewer parking spaces may be proposed subject to the applicant submitting a parking needs assessment prepared by a Connecticut licensed professional engineer addressing how the following criteria supports the lesser parking ratio without impact health and safety:
 - (1) available existing public and private parking that may be used by residents of the proposed development,

- (2) public transportation options that may be used by residents of the proposed development that mitigate the need for off-street parking,
- (3) projected future needs for off-street parking for such proposed development, and
- (4) any relevant local traffic, parking or safety study.

In the case of mixed uses, uses with different parking requirements occupying the same building or premises, the parking spaces required shall equal the sum of the requirements of the various uses computed separately unless the Decision-Making Authority determines that the plan meets the requirements of Section 18.2.2 of these Regulations.

18.3.4 For Business or Professional Office or Governmental Office Space: One (1) space for each two hundred fifty square feet (250sqft) of gross floor space.

18.3.5 Retail Stores and Restaurants containing drive through facilities:

- Pick up facilities / windows which include required stacking lanes, shall be located in the rear or side yards only. Provisions shall be made to park the minimum stack of vehicles between the street line and the drive through window, with provisions also made to allow at least one exiting automobile to park between the window and the street. The space at the pick-up window shall not be counted as a stacking space.
- Stacking lanes shall be a minimum of ten feet (10') wide and twenty feet (20') long.
- Stacking lanes shall be separate from internal aisles, which allow traffic to circulate through the site without entering the drive-through lane.
- Stacking lanes shall be designated and located so as to minimize traffic congestion and to promote pedestrian safety through the use of pavement markings, signs, and designated walkways.
- Drug Stores: A minimum of three (3) stacking spaces.
- Bank: A minimum of five (5) stacking spaces.
- Car Wash: A minimum of five (5) stacking spaces and two (2) stacking spaces at car wash bay exit.
- Restaurant Fast Food: A minimum of five (5) stacking spaces.

18.3.6 For Retail Stores less than twenty thousand square feet (20,000sqft) of gross floor area, Personal Service Shops, Repair Shops and similar Commercial

Uses: One (1) space for each two hundred fifty feet square feet (250sqft) of gross floor area. For Retail stores or similar uses greater than twenty thousand square feet (20,000sqft), the applicant shall submit a parking plan designed by a Connecticut Licensed Professional engineer.

- 18.3.7 Retail – Convenience Store Gas Sales: Five (5) parking spaces per one thousand (1,000) square feet of gross floor area. The number of required spaces may be reduced by the number of fuel pumps that can be accessed at any one time. There shall be one (1) space for each three (3) interior on-site seats. There shall be one (1) space for each two (2) employees. There shall be one (1) space adjacent to a vacuum facility or tire air station if one is provided. In no event shall the number of required spaces be less than three (3) per one thousand gross square feet (1,000sqft) of floor area. In no instance shall a required parking space or its maneuvering area conflict with vehicles being fueled or awaiting fuel.
- 18.3.8 For Furniture, Machinery, Equipment, Automobile and Boat Sales and / or Service Establishments: One (1) space for each four hundred square feet (400sqft) of gross floor area, and one (1) space for each eight hundred square feet (800sqft) of outdoor sales and/or displayed area.
- 18.3.9 For Restaurants, Night Clubs, Bars and Lounges: One (1) space for each three (3) persons who may legally occupy the facility where allowable occupancy is determined in accordance with Section 18.4.4 and three (3) spaces for Pick-Up Service.
- 18.3.10 For Fast-Food Restaurants or Other Food Service Establishments Where Customers are served by primarily counter service: One (1) space for every three (3) permanent seats and one (1) space for each nine (9) square feet of public floor area not devoted to permanent seating facilities, excluding restrooms. Where any drive-in service window is established provisions shall be made to park at least five (5) waiting automobiles between the street line and the drive-in window being approached, with provisions also made to park at least one (1) exiting automobile between said window and the street. Such facilities shall be provided in such a way as to cause no interruption to the smooth flow of traffic within the subject site.
- 18.3.11 For Theaters, Grandstands and Stadiums, Auditoriums or Meeting Rooms and Similar Places of Public Assembly: One (1) space for every three (3) seats if permanent seating is provided, or one (1) space for each twenty-one (21) square feet of public area for areas not served by permanent seating.

- 18.3.12 For Hotels, Motels, Lodging, or Boarding Houses: One (1) space for each room, plus spaces as required for other related uses, plus one (1) space for each two (2) employees.
- 18.3.13 Hospitals: One (1) space for each bed plus one (1) space for each two (2) employees on the single largest shift.
- 18.3.14 Nursing and/or Convalescent Homes: One (1) space for each three (3) patient accommodations plus one (1) space for each two (2) employees in the single largest shift.
- 18.3.15 For Industrial Plants, Wholesale Establishments, Warehouses and Similar Buildings: One (1) space for each one thousand square feet (1,000sqft) of floor area or one (1) space for each three (3) persons normally employed, whichever is greater.
- 18.3.16 Buildings Occupied by a Government Unit or religious Institution: One (1) parking space for each three (3) persons for which seating accommodations are provided, plus parking as required for related uses as specified herein.
- 18.3.17 Private Clubs, Fraternities, Sororities, Country Clubs, or Other Similar Organizations: One (1) space for each two (2) employees, plus spaces as required herein for restaurant areas, places of assembly, etc. If sleeping accommodations are provided, parking shall be provided in addition to other requirements at a standard of one (1) space per sleeping room, or if dormitory accommodations are provided, at one (1) space for each two (2) beds. In those cases where golf courses are involved, parking shall be provided for at least one hundred (100) cars, and where tennis facilities are involved, parking shall be provided for at least four (4) cars per court.
- 18.3.18 Commercial Recreational Facility: One (1) space for three (3) users who could be utilizing the premises at any one single time plus one (1) space for each three (3) seats provided for spectator observance of the establishment's activities. If restaurant or other commercial facilities are included in the facility, additional parking shall also be provided in accordance with the requirements for such related uses as specified herein.
- 18.3.19 Automotive Services Including but not Limited to Gas Stations, Auto dealers, Auto Accessories, Auto Repair, Auto Body and Paint Shop, Muffler Installations, Tire Shops, Engine and Transmissions Overhaul Shops Excluding Convenience Store Gas Sales: Five (5) spaces plus two (2) spaces for each service stall, plus two (2) spaces for each pump. In addition, if motor vehicles are offered for sale and / or rent on the premises, one space shall be provided for each such vehicle offered for sale and /or rent. Furthermore, an

attendant operated or self-service car wash shall have at least ten (10) waiting positions for each bay between the street line and such bay for cars approaching, and at a least two (2) waiting positions for cars leaving such bays.

18.3.20 Medical or Dental Offices: One (1) space for each one hundred fifty square feet (150sqft) of gross floor area.

18.3.21 Mixed Uses: A parking study in accordance with Section 18.3.2.b shall be required for mixed-use developments that include more than sixteen (16) residential dwelling units in combination with one or more non-residential uses, whether the residential dwelling units or other uses are existing or proposed.

18.4 INTERPRETATION OF OFF-STREET PARKING

18.4.1 The Parking required herein is in addition to space which is required for the storage of trucks or other vehicles used in connection with a business, commercial or industrial use.

18.4.2 Where fractional spaces result, the parking spaces required shall be construed to be the next highest whole number.

18.4.3 The parking space requirements for a use not specifically listed in this section shall be the same as for a listed use of similar characteristics of parking demand generation, as determined by the Decision-Making Authority . For uses not specified, the Applicant must supply the industry or business standard or a plan signed and sealed by a Licensed Professional Engineer certifying sufficient parking.

18.4.4 In those instances where the parking requirements related to the number of people using a particular building or portion thereof, the occupancy allowances published in the State Building Code or Fire Code shall govern whichever is more restrictive.

18.5 LOCATION OF REQUIRED PARKING SPACES

1. On-site. Required parking facilities shall be located on the same lot as the dwelling unit, building, or on the use which they serve except as noted in these Regulations. The applicant must provide the minimum number of parking spaces required by these Regulations. The Decision-Making Authority shall limit the number of spaces to those required by these Regulations for the proposed use except as noted in these Regulations.

2. Off-site. The Decision-Making Authority may approve parking on a noncontiguous lot if the Applicant supplies sufficient evidence that the lot is under the control of the Applicant/Owner by way of easement, lease or ownership. It shall be the Applicant/Owner responsibility to maintain easement, lease or ownership to run concurrent with the approved project.

3. Front Setback. Parking and drives shall be setback twenty-five (25) feet from Front Property Lines.
4. Side and Rear Setback. Parking and drives shall be setback ten (10) feet from Side and Rear Property.

18.6 DRIVE AISLES

Drive Aisles shall be of adequate width to serve a particular design arrangement of parking spaces, the following being the minimum width permitted. Ninety degrees (90°) parking shall be used unless there is positive control of traffic directions.

- 90° parking – twenty-four feet (24')
- 45° parking – thirteen feet (13')
- 60° parking – eighteen feet (18')
- 30° parking – eleven feet (11')

18.7 SURFACE OF PARKING AREAS

All off-street parking areas shall be surfaced and maintained with a durable and dustless all-weather material as may be approved by the Decision-Making Authority . Parking areas shall be so graded and drained as to dispose of all surface water and proposed surfacing and drainage plans shall be submitted to and approved by the Decision-Making Authority . In no case shall drainage be allowed across any sidewalk areas.

18.8 LANDSCAPE ISLANDS, ACCESS DRIVES AND VEHICULAR CIRCULATION

Provisions shall be made for vehicular access to the lot and circulation upon the lot in such a manner as to safeguard against hazards to traffic and pedestrians in the street and to provide safe and convenient circulation in the street and upon the lot.

All parking areas shall be designed in such a manner that any vehicle entering or leaving the parking area from or to a public or private street shall be traveling in a forward motion.

All access driveways for parking areas and / or loading areas shall be located in such a way that any vehicle entering or leaving such areas shall be clearly visible for a reasonable distance to any pedestrian or motorists approaching such driveway from a public or private street.

All driveways providing connection between any parking and / or loading areas and any public street or public right-of-way shall be finished with bituminous concrete or concrete which shall conform with the Town of Montville Road Standard and Improvement Details as may be amended from time to time.

- 18.8.1 All parking areas shall include islands and medians as set forth in Section 18B Landscape Design Standards.

- 18.8.2 The entrances and exits to all parking areas shall be clearly marked. Access drives for one way traffic shall have minimum width of fourteen feet (14') where one way traffic is anticipated and a minimum width of twenty-four feet (24') traffic is anticipated.
- 18.8.2 All parking spaces in parking areas of more than fifteen (15) cars and all loading spaces shall be so located that vehicles entering or leaving such spaces do not block any entrance drive to the parking facility. In addition, all parking spaces shall be entered and exited along interior drives arranged perpendicular to access drives wherever possible. No parking shall be permitted along access drives within forty feet (40') of the street line and at greater distances as may be required by the Decision-Making Authority depending on the traffic generation and parking lot size. Dead end parking aisle interior drives shall be extended ten feet (10') further than the last parking space to allow movement of a vehicle in and out of a parking space.
- 18.8.3 All exits and entrances shall be so located as to provide the least amount of interference with the movement of pedestrian and vehicular traffic. Each entrance and exit shall be at least twenty feet (20') distance from any residential property or residential district and at least seventy-five feet (75') distance from any street intersection, except entrance and exit onto a collector or arterial street which shall be one hundred fifty feet (150') from any intersection and where traffic circulation would be improved by having entrances and exits directly opposite to existing curb cuts or roads.
- Decision-Making Authority Decision-Making Authority
- 18.8.4 Where a lot has frontage on two (2) or more streets, the access to the lot shall be provided to the lot across the frontage to the street where there is lesser potential for traffic congestion and for hazards to traffic and pedestrians.
- 18.8.5 The street giving access to the lot shall have traffic carrying capacity and be suitably improved to accommodate the amount and types of traffic generated by the proposed use.
- 18.8.6 When necessary to safeguard against hazards to traffic and pedestrians and / or avoid traffic congestion, provision shall be made for turning lanes, traffic directional islands, frontage roads, driveways, and traffic controls within the streets.
- 18.8.7 Where topographic and other conditions are reasonably useable provisions shall be made for circulation driveway connections to adjoining lots of similar existing or potential use; (1) when such driveway connection will facilitate fire protection services as approved by the Town Fire Marshal and / or; (2) when such driveway will enable the public to travel between two existing or potential uses, open to the public generally, without need to travel upon a street.
- 18.8.8 Where the lot has frontage on an existing street, proper provision shall be made for grading and improvement of shoulders and sidewalk areas within the

right-of-way of the street and for provision of curbs and sidewalks, as approved by the Decision-Making Authority and in accordance with the pattern of development along the street. Where necessary to provide for suitable access for a system of neighborhood circulation streets, provision shall also be made for appropriate continuation and improvement of streets terminating at the lot where the use is to be located.

18.9 LANDSCAPING REQUIRED

All Parking areas shall conform to Section 18B Landscape Design Standards.

18.10 PROPERTY MAINTENANCE

The owner of property used for parking and / or loading shall maintain such area and all required sidewalks and buffer areas in good conditions without holes and free of all dust, trash, and other debris.

18.11 CURBING REQUIREMENTS

Appropriate provisions shall be made to prevent vehicles from overhanging walkways and from damaging trees or other landscaping materials. Furthermore, whenever a parking lot extends to a property line, wheel blocks or other suitable devices shall be installed to prevent any part of a parked vehicle from extending beyond such property lines.

18.12 FIRE LANES

No parking shall be permitted in those areas designated as fire lanes on any site plans approved under the provisions of these Zoning Regulations or established by the Town's Fire Marshal.

18.13 MARKING

All required parking spaces and fire lanes, except spaces required for one (1) family or two (2) family dwellings and other approved alternative parking areas, shall be marked by painted lines, maintained in good condition, curbs, or other means to indicate individual spaces. Signs or markers painted or provided and maintained in good condition shall also be used as necessary to insure efficient traffic flow within all parking lots and between any such lot and the public street or right-of-way serving such lot.

18.14 LIGHTING

All parking areas shall conform to Section 18C Lighting Design Standards.

Decision-Making Authority 18.15 HANDICAPPED PARKING SPACES

Handicapped parking spaces shall be provided for in accordance with the provisions of the Building Code of the State of Connecticut and shall be included in the total required parking spaces.

Decision-Making Authority

NEW

18B LANDSCAPE DESIGN STANDARDS

18B.1 Purpose

The Commission affirms these standards are necessary to protecting the health, safety and welfare of the town of Montville as landscaping:

1. Improves sites appearance and contributes to community and neighborhood identity;
2. Increases shade to reduce heat island effect, reducing environmental impacts;
3. Provides habitat for wildlife and migratory birds;
4. Provides opportunity for low impact development options and stormwater runoff quantity and quality;
5. Supports safe vehicular and pedestrian circulation with appropriate use and placement;
6. Creates effective buffers between land uses.

18B.2 Applicability

The minimum landscaping requirements established in this section apply to:

1. Zoning permits other than single family and duplex units;
2. Site plans and special permit applications;
3. When there is a change of use which requires less parking than exists, the number of reduced parking shall be removed and replaced as follows, listed in order of priority:
 - a. End Islands
 - b. Intermediate Islands
 - c. Median Areas
4. When there is a change of use with no effect on parking, end islands shall be added at the end of each parking row when the Decision-Making Authority determines there will be no adverse impact from the reduction of parking.
5. Parking areas which undergo pavement reclamation of more than 50% of the Parking Area shall provide at minimum End Islands when the Decision-Making Authority determines there will be no adverse impact from the reduction of parking.

18B.3 General Standards

1. Refer to Landscape related terms for all definitions;

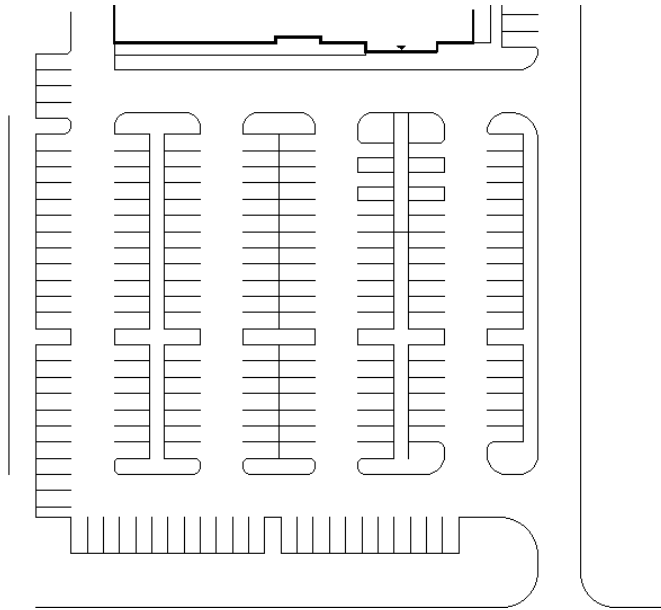
2. All required landscape plans shall be prepared, signed, and sealed by a Connecticut-licensed Landscape Architect;
3. Plant measurements, stock and planting specifications shall follow AmericanHort standards;
4. Plantings shall provide shade, define circulation, reinforce pedestrian connections, and maintain safe sight lines;
5. Plant materials shall be selected for durability, salt tolerance, and suitability for parking-lot growing conditions;
6. Shade Trees and Street Trees shall be a minimum caliper of 2 to 2.5 inches at time of planting;
7. Evergreen Trees shall be a minimum height of six-eight feet at time of planting;
8. Shrubs shall be a minimum size of 18-24" height/spread at time of planting;
9. The Decision-Making Authority may allow an alternative plant size based upon the application of vegetation, plant availability and species;
10. Invasive plant materials identified by the CT Invasive Plant List as maintained by the CT Invasive Plant Council in accordance with 22a-381a through 22a-381d or prohibited;
11. All plant material shall be hardy to the USDA Plant Hardiness Zone applicable to the Town as determined by the most current USDA Plant Hardiness Zone Map;
12. All plantings shall be coordinated with the overall lighting plan to avoid conflicts of lighting and planting placement.

18B.4 Locating Plant Material and Conflicts with Existing and Proposed Conditions

1. General. All plant material shall be located and selected based upon its intended use and not interfere with pedestrian or vehicular movements;
2. Sight Lines. All plant material shall be selected, located and sized to not conflict with sight lines at intersections of roadways, driveways and drive aisles;
3. Pedestrian Visibility. All plant material shall be selected, located and sized to not conflict with visibility of pedestrians entering and within crosswalks;
4. Existing Conditions. Should there be a conflict with existing utilities, steep slopes or other conditions and which are not feasibly capable of being adjusted and prohibits meeting the requirements of this Section, all required landscaping for said location shall be provided within the property at another location on site as determined suitable by the Decision-Making Authority;
5. Proposed Utilities and Site Grading. Landscaping shall take priority over proposed utility infrastructure siting and routing and the establishment of final grades. However, if the Decision-Making Authority determines the necessity of a particular utility location or routing, or the creation of steep slopes from proposed grading, landscaping may be

omitted or reduced at the affected area(s) and shall be provided within the property at another location on site as determined suitable by the Decision-Making Authority.

18B.5 Parking Area Landscaping (Labels to be provided and will be larger)



1. General Standards
 - a. Landscape islands shall align to each other when parking rows are adjacent or opposite one another as to maintain orderly circulation and continuous planting areas;
 - b. Where islands or medians are designed as low-impact development (LID) areas to collect and treat stormwater, curbing may be omitted to allow sheet flow, subject to approval by the Decision-Making Authority and the Town Engineer.

2. Landscaped End Islands
 - a. Landscaped End Island shall;
 - i. be of a dimension equal to the parking stall dimension used to achieve the purpose of these regulations;
 - ii. be provided at the end of each parking row;
 - iii. contain at least one deciduous Shade Tree;
 - iv. be planted with Groundcover Vegetation;

- v. be hardscaped only if utilized for pedestrian circulation for a crosswalk serving a main entrance to a building or clearly distinguished desire line.
- b. The Decision-Making Authority may require larger Landscaped End Islands to accommodate appropriate turning radius at locations of high anticipated vehicle or pedestrian traffic.

3. Landscaped Intermediate Islands

- a. A Landscape Intermediate Island shall:
 - i. be provided for parking rows exceeding twenty spaces at a rate of one (1) island for every twelve (15) parking stalls within each row (minimum one island);
 - ii. be of a dimension equal to the parking stall dimension used to achieve the purpose of these regulations.
 - iii. contain at least one deciduous Shade Tree;
 - iv. be planted with Groundcover Vegetation.
 - v. be hardscaped only if utilized for pedestrian circulation for a crosswalk serving a main entrance to a building or clearly distinguished desire line.

4. Landscaped Median Islands

- a. Landscaped Median Island shall;
 - i. be provided for every four (4) parking rows and shall be centrally and evenly spaced across the parking area;
 - ii. have a minimum clear width of eight (8) feet measured from back of curb;
 - iii. contain Shade Trees spaced at intervals of one (1) tree for every ten (10) parking stalls with a minimum of one Shade Tree
 - iv. may be partially hardscaped only if utilized for pedestrian circulation for a crosswalk serving a main entrance to a building or clearly distinguished Desire Line.
- b. Landscaped End Islands and Intermediate Islands planting requirements shall not be used to satisfy Landscape Median Island planting requirements.
- c. Landscaped Medians Islands are encouraged to be designed as low-impact development (LID) areas to collect, infiltrate, and treat stormwater.

5. Drive and Perimter Landscaping

- a. There shall be one Shade Tree for every 10 parking spaces located along the perimeter of a Parking Area.

- b. Shade Trees shall be planted along both sides of drives at the rate on one Shade Tree for every fifty (50) feet.

6. Parking in Front Yard

When parking is located in the Front Yard a landscaped buffer, in addition to the Street Trees required in Section 18B.9 Frontage and Streetscape Landscaping, shall be provided for every 40 feet of frontage, or increment, as follows:

- a. One (1) deciduous shade tree;
- b. Twenty-five (25) shrubs, evergreen or deciduous (quantity proportional to frontage length); and
- c. A three-foot-high landscaped berm.

18B.6 Site Retaining Walls.

The following applies to all retaining walls visible from a public right of way or approved residential use or zone:

- 1. Retaining walls 4' or greater shall be planted at its base with a continuous planting bed of woody ornamental evergreen and deciduous shrubs. Herbaceous ground cover is permitted when in combination with shrub planting.
- 2. Retaining walls 8' and higher shall be terraced with an intermediate terraced planting bed of 6' in depth measured between the back of the lower wall coping and base of the upper wall shall be planted at its base with a continuous planting bed of woody ornamental evergreen and deciduous shrubs.
- 3. Sections of Retaining walls which are utilized for site signage as approved under these regulations may be allowed to reduce the landscape requirement for the length of the approved sign.

18B.7 Side and Rear Yard Perimeter Buffers **needs work reconcile with 18.6.2**

1. Complete Visual Landscape Buffer

- a. A full landscape buffer of ten (10) feet shall be provided along any side or rear property line abutting a residential, civic, or institutional district or use. The buffer shall include:
 - i. Deciduous shade trees; and
 - ii. A continuous evergreen screen.

2. Partial Visual Landscape Buffer

- b. A partial landscape buffer of ten (10) feet shall be provided along any side or rear lot line abutting a non-residential district or use. The buffer shall include:
 - i. Deciduous shade trees; and
 - ii. Evergreen and/or deciduous shrubs.

18B.8 Frontage and Streetscape Landscaping

- 1. Street Tree Quantity. There shall be one (1) Street Tree for every forty (40) feet of frontage or fraction thereof.
- 2. Street Tree Location. Street Trees shall be planted within 3' of the front Property Line.

18B.9 Stormwater Management Areas (SWMA)

- 1. Surface stormwater management areas shall, to the extent feasible, not be located in Front Yards.
- 2. When visible from public rights of way or residential districts, stormwater management areas shall be landscaped with native plant materials compatible with the SWMA characteristics, integrated in the over all landscape and includes a mix of herbaceous, seed mixes, shrubs and trees.
- 3. In addition to above, SWMAs in rear and side yards shall be planted with a Complete Visual Landscape Buffer along side and rear yards.

18B.10 Bonding and Maintenance

- 1. A landscape bond shall be submitted for the value of all landscaping for the approved plan or initial phase at time of CZC.....**???**
- 2. All required landscaping shall be maintained in healthy condition. Dead, damaged, or diseased plantings shall be replaced with materials of equivalent size and type.

NEW

18C LIGHTING STANDARDS

18C.1 Purpose

The Commission affirms Site Lighting is vital to protecting the health, safety and welfare of the Town of Montville. When not appropriately used, lighting is detrimental to the environment and wildlife habitat, is a nuisance to neighboring property owners, has impacts on human health due to lighting levels and color, has negatively reduced the enjoyment of the night sky, among other impacts. This regulation helps to reduce these negative attributes.

18C.2 Applicability

1. This regulation applies to all site and exterior building mounted fixtures as follows:
 - a. Site Plan Applications
 - b. Replacement of existing fixtures, in which case only those regulations apply which are influenced by the luminaire.
2. Uses exempt from this regulation are:
 - a. Single family homes
 - b. Duplex homes
 - c. Traditional seasonal lighting remaining no longer than 60-days;
 - d. Temporary lighting (lasting no longer than 10 days) for fairs, carnivals or similar functions only when authorized by the Town;
 - e. Town of Montville Government and safety facilities (does not include Board of Education uses or facilities).

18C.3 Lighting Layout and Plan Required

1. Reference. The standards used herein shall follow, to the extent practical, the best practices of the Illuminating Engineering Society of North America (IESNA).
2. General. The layout of all luminaires shall be coordinated with site features, such as but not limited to landscaping, buildings and structures, utilities, grading and slopes, and stormwater features.
3. Parking. Lighting shall be located in islands and medians to the extent feasible. Lighting within open parking areas shall be located along striping which designating parking rows, not located within parking stalls.
4. Setbacks. Lighting is permitted within setbacks and shall meet all standards set forth.
5. Essential/Non-essential Lighting Plans. Two photometric plans shall be prepared, one depicting essential lighting and the second depicting non-essential/security lighting and shall be by a Connecticut licensed professional engineer or landscape architect as follows:

- a. depicting site and building lighting locations,
 - b. key/legend identifying the type of fixture and mounting height
 - c. foot-candle distribution throughout the site and along adjacent properties.
6. Lighting Height. Light pole height is measured from the finished grade at the light pole base to the top of the luminaire.

18C.4 Light Trespass, Pollution and Glare Control

1. All luminaires shall have a BUG (Backlight, Uplight, Glare) rating which aligns with the intended use of the light fixture as defined by IESNA.
2. All lighting shall be full cut-off type fixtures, fully shielded/recessed and selected and installed to not permit light trespass, and sky pollution.
3. No light shall be directed over a property line.
4. Lighting shall be fully shielded, emit no light above 90 degrees from Nadir, and shall not exceed 0.1 foot candles at any property line.
5. All fixtures shall be shielded and directed as to not cause glare, whether glare is direct or reflected from a surface, such as wall, roof, siding, sign or pavement.

18C.5 Light Pole Foundation and Height; Parking Areas

Light poles shall not exceed 24'; except in R-20, R-40, R-80 and R-120 zones, light poles shall not exceed 20'. Foundations shall not be exposed more than 18" measured from finished grade.

18C.6 Light Pole Foundation and Height; Walkways and Courts

Light poles adjacent to walkways and within courts/entries shall not exceed 16'. Foundations shall be flush to the finished grade and screened by the light pole base.

18C.7 Building Mounted Lighting

All building mounted lighting shall be incorporated into the overall lighting plan and location clearly depicted. Lighting used solely for building code purposes shall be to the level to meet the minimum applicable building code.

18C.8 Fuel Station Canopies

Fuel Station Canopy Lighting must be recessed or flushed with the underside of the canopy. Specific horizontal illuminance levels under the canopy must be provided as part of the Lighting Plan.

18C.9 Color Temperature (Kelvin Limits)

Lighting color temperature shall not exceed 3000 Kelvin for all uses.

18C.10 Outdoor Recreation and Activity Areas

Where outdoor playing fields or other special outdoor activity areas are to be illuminated, lighting fixtures must be specified, mounted, and aimed so that:

1. The lighting shall be designed to meet the specific needs of the recreational activity; and
2. The lighting is full cut-off or IESNA cut-off fixtures, as approved by the Decision-Making Authority. Any application for use of IESNA fixtures should include “cut-sheets” with specifications and details of the proposed fixture and pole.
3. The lighting meets ANSI/IES RP-6-22 Lighting Sports and Recreational Areas requirements, as amended.
4. All power for field lighting shall be reduced by at least 75% one hour after the conclusion of each activity and be turned off within two hours after the conclusion of the activity.

18C.11 Aesthetics, Sign and Display

Lighting may be used to highlight architectural features, signs or displays. Light fixtures shall:

1. Be downward oriented; up or sideways lighting is prohibited;
2. Be full cut-off;
3. Configured with photocell or time clock on – off and be turned off within one hour after business closes.

18C.12 Flag Lighting

Lighting to illuminate the American flag only at night may be used, when the flag is building mounted no higher than twenty (20) feet or pole mounted no higher than thirty (30) feet and provided the light fixture meets the following requirements:

1. Is mounted at, or below ground level within a seven (7) foot radius of the pole,
2. Is designed to cast a narrow beam focused directly at the flag, and
3. Is fully shielded to prevent glare or light trespass to surrounding areas.

18C.13 Security and Time of Lighting Operations

1. All non-essential lighting shall be photocell-on with time-lock off one hour after business close. Essential lighting used for security shall be reduced down to the minimum level required by insurance underwriting. Applicant shall provide evidence of such minimum lighting level from the insurance underwriting agency.

2. Lighting plan shall identify essential v. non-essential lighting.

18C.14 Prohibited Lighting

The following types of lighting are prohibited, unless otherwise exempted from these provisions:

1. Mercury vapor lamps;
2. Unshielded lighting including 'flood lights';
3. Searchlights, beacons, and laser source light fixtures, including when directed upward at such an angle that neither buildings, trees, shrubs or site surfaces are lit;
4. Animated, flashing, moving and festooned lighting and lighting producing changing color.

New Section

18D. SITING OF NECESSARY SITE APPURTENANCES

18D.1 Purpose

The Commission affirms appurtenances can detrimentally impact safety and welfare when not properly located. Therefore, this section sets forth the minimum criteria for locating, screening, noise attenuation and reducing or eliminating visible impacts.

18D.2 Applicability

1. This section establishes minimum standards for all new, replacement of and modification to existing exterior Utility Appurtenances which are visible from a public right of way, residential zone or use.

18D.3 Location on Site

1. All other Site Appurtenances are prohibited within front, side and rear setbacks.
2. Trash and Recycling Enclosures are prohibited from the Front Yard.

18D.4 Conflicts

These standards may be modified when the applicant demonstrates with substantiated evidence that these standards would be in conflict with public utility provider's standard.

18D.5 Screening and Openings

1. Small Site Appurtenances less than four (4) feet in height, or located no higher than four (4) feet above finished grade shall be screened with evergreen plantings no less than the height of the Appurtenance.
2. Large Site Appurtenances four (4) feet or higher shall have an opaque masonry wall or fencing screen equal to or greater than the height of the Appurtenance. Landscaped plantings comprised of evergreen and deciduous shrubs of a height at maturity equal to or greater than ½ the opaque screen height.
3. Large Site Appurtenances access openings visible from a public right of way or adjacent property must be furnished with opaque gates.

18D.6 Roof Mounted Equipment

Roof mounted equipment shall be completely screened from view from all public rights of way through the utilization of parapets or similar means which are architecturally integrated into the building's design.

18D.7 Wall Mounted Equipment and Meters

Wall mounted equipment and meters are prohibited from facades facing the public right of way.

18D.8 Above Grade Utility Conduit

Utility conduit service lines which cannot be placed below grade and entering a building shall be integrated into the design of the building façade through color selection or architectural detailing.

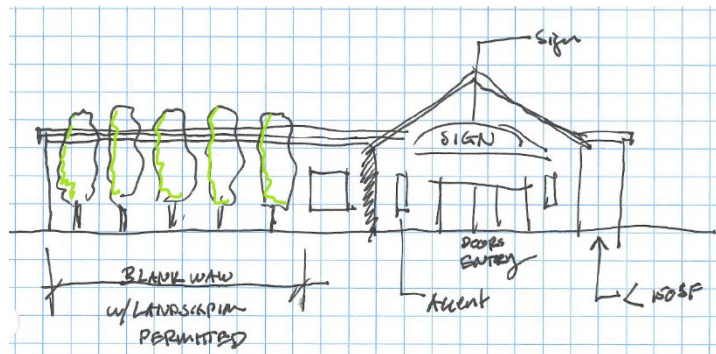
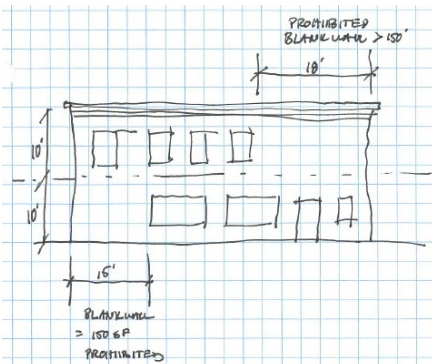
18D.9 Noise Standards

With the exception of Site Appurtenances used for emergency purposes only, all Appurtenances shall not emit noise exceeding the standards of CGS 22a-69-1 – 22a-69-7.4.

NEW

18E. BLANK WALL AND USE OF FENESTRATIONS

1. A Blank Wall is prohibited when visible from public rights of ways and abutting residential uses or districts.
2. Two or more Architectural Façade Elements may be used when the use of Fenestrations is not feasible. One Architectural Façade Element may be used which is equal the length of the Blank Wall and one-quarter of the wall height.
3. Landscaping may be utilized to divide ground floor Blank Walls in lieu of Architectural Façade Elements. When landscaping is used there shall be one shrub or tree placed within ten (10) feet of each fraction of a Blank Wall and which mature height is equal to or greater than the top of the Blank Wall.
4. One sign in conformance with these regulations may be allowed meet the requirements of section



NEW

18F. SNOW STORAGE AND REMOVAL

1. A snow storage plan for snow events of six inches shall be provided which:
 - a. Depict the limits of snow removal with supporting calculations to determine snow volume (in cubic FT).
 - b. Depict the limits of snow storage with supporting calculations confirming quantity of snow storage achieved. For the purposes of this calculation, a snow storage area is an area where the slope of the snow being stored is no greater than one foot vertical to one foot horizontal (1:1).
2. If offsite snow removal is intended, a removal plan shall be provided which describes methods and location of disposal.
3. Snow Storage Areas shall not:
 - a. Impede sight lines.
 - b. Be located within planting/landscaping areas and placed in a manner which detrimentally effects the health of landscaping.
 - c. Located in designated parking stalls and loading zones.
 - d. Disposed of within stormwater management areas.
 - e. Be located to impede handicap accessible routes and building egress.
4. To the greatest degree possible, be located to ensure snow melt receives stormwater treatment within the stormwater management system.

	Montville	East Lyme	Groton City	Groton Town	Ledyard	Waterford	NL	Norwich	Stonington
Zoning Permits: Fees do not include \$60 State Application Fee							\$25 for all		
Dwelling Units:									
Single Family Residence (SFR)	\$50.00		Requires site plan application		\$100.00	\$110.00		\$50.00	\$250 & \$25 per 100 SF gross flr area
New SFR, including new units of mulit-family dwellings fee is per unit		\$300.00							
SFR Alterations, addition under 1,000 sq ft	\$25.00	\$100.00							\$50 & \$10 per 100 SF gross flr area
SFR Alterations, addition over 1,000 sq ft	\$25.00	\$200.00							\$200 & \$10 per 100 SF gross flr area
Multi-family	inc in PZC					\$10 per \$1000 of value of site imp. work and E&S		\$200.00	
Subdivision	inc in PZC					\$55.00			
Accessory Dwelling Unit	\$25.00					\$110.00			
Accessory Apartment						\$50.00			
Detached Garage									\$100.00
Renovations									\$50.00
Other new construction/reconstruction									\$50.00
Accessory Uses:	\$25.00					\$55.00		\$25.00	
Sheds Under 200 sq.ft	\$0.00								\$25.00
Sheds Greater than 200 sq ft	\$25.00								\$100.00
Swimming Pools	\$25.00				\$40.00				\$25.00
Accessory Structure over 150 sf		\$50.00							
Accessory Structure under 150 sf		\$20.00							
Accessory Structure /Addition up to 200 SG					\$25.00				
Accessory Structure/Additions 200 - 1000 SF					\$40.00			\$25.00	
Accessory Structure/Additions over 1000 SF					\$100.00				\$200.00
Temporary Structure						\$55.00			
Residential Grade & Fill						\$55.00			
New Business Registration:									
Commercial/Industrial	50	50							
Home Occupations	\$25.00	\$25.00			\$50.00	\$55.00		\$25.00	\$50.00
Home Office	\$0.00								
Commercial Uses:	\$50.00				\$50.00			\$200.00	
<i>New commercial/institutiona buildings, plus site plan review fee and storm water review fee</i>									
Over 25,000 sq ft, plus \$10 per sq ft gross floor area		\$2,000.00							
Up to 25,000 sq ft		\$500.00							
<i>Commerical/industrial/institution additions , plus site plan review fee and storm water review fee</i>									
Over 1500 sq feet		\$300.00							
Under 1500 sq ft		\$100.00							
<i>Commerical/industrial/institution alterations</i>									
Under 1500 sq ft		\$100.00							
Over 1500 sq feet		\$200.00							
Conversion Permit								\$100.00	
Signs	\$50.00				\$25.00	\$55.00	\$25.00		
up to 18 SF									\$25.00
greater than 18 SF									\$100.00
New multi-tenant signage program									\$250.00
Modification to multi-tenant signage program									\$110.00
Letter of Zoning Compliance	\$75.00	\$50.00	\$50.00	\$25.00	\$50.00	\$55.00	\$50.00	\$20.00	
Lot Split	\$0.00	\$200.00			\$50.00	\$110.00			

	Montville	East Lyme	Groton City	Groton Town	Ledyard	Waterford	NL	Norwich	Stonington
Planning & Zoning Commission Applications									
Site Plan:									
Residential Single Family	n/a		\$80.00		\$200.00				\$200.00
Accessory Structures to SFR over 1200 SF	\$150.00								
Multi-Family							\$350.00	\$300 over 10 units	\$200 and \$50 per 1,000 sf
Commercial/Industrial/Mixed Use	\$150.00	\$300.00			\$300.00	\$300.00	\$500.00	\$200.00	
up to 5,000 SF									\$200 and \$50 per 1,000 sf
5001 - 25000 sf									\$500 and \$50 per 1,000 sf
New Construction <25,000 sq ft (plus)			\$200.00	\$120.00		\$200.00			
25,000 - 100,000 sq ft (plus)			\$400.00	\$220.00		\$300.00			\$2,000 & \$10 per 1,000 sf
>100,000 sq ft (plus)			\$500.00	\$320.00		\$400.00			
Site Plan Review base and \$25/1000 sf							\$200.00		
Facade Review							\$50.00		
Temporary Outdoor Entertainment							\$25.00		
Earth Extraction/Fill							\$350.00		
Modified Site Plan:									
Commission Approval	\$150.00		\$125.00	\$120.00		\$300.00	\$50.00		
Administrative Approval	\$0.00		\$50.00						
<25,000 sq ft						\$100.00			
25,000 - 100,000 sq ft (plus)						\$150.00			
> 100,000 sq ft (plus)						\$200.00			
Stormwater Review		\$300.00							\$250.00
Special Permit:	\$450.00		\$250.00	\$370.00	\$500.00	\$300.00	\$250 & \$25/1000 sf	\$500.00	
<25,000 sq ft (plus)						\$200.00			
25,000 - 100,000 sq ft (plus)						\$300.00			
> 100,000 sq ft (plus)						\$400.00			
Attached housing of any size, Comm, Ind up to 5000 sf									\$500 and \$100 per 1,000 sf
Comm/ind 5001 - 25,000 sf									\$1,000 and \$50 per 1,000 sf
Comm/ind 25,001 and greater									\$2,000 and \$25 per 1,000 sf
not including site plan & Stormwater review fees		\$500.00							
Legal notice						actual cost			
Zoning Reg Amendment:	\$450.00	\$300.00	\$250.00	\$370.00	\$500.00	\$300.00	\$300.00		\$500.00
Legal Notice						actual cost			
Zone Map Amendment:	\$450.00	\$400.00	\$250.00	\$370.00	\$500.00	\$300.00	\$300.00		\$250 and \$100 per lot
Legal Notice						actual cost			

	Montville	East Lyme	Groton City	Groton Town	Ledyard	Waterford	NL	Norwich	Stonington
Subdivision:									
Base Fee		\$750.00		\$370.00	\$250.00	\$300.00	\$500.00	\$300.00	\$500.00
Per Lot Fee	\$50 per lot	\$100.00	\$125.00		inc 3 lots	\$100.00	\$150.00	\$100.00	Lots: 2-20 \$225, 21-50 \$200, 51+ \$175
Per lot 1-4				\$225.00					
Per lot 5-49				\$150.00	\$50.00				
Per lot 50 and over				\$125.00					
Professional/legal consultations		Actual Cost	150% of cost						
Road/utility design review fee		\$1/linear ft of new road							
Modification for sub/re-sub				\$220.00	\$500.00		\$150.00		
ReSubdivision:									
Base Fee	\$450.00	\$1,250.00		\$370.00	\$750.00	\$300.00	\$500.00	\$450.00	
Per Lot Fee	\$50.00		\$125.00		inc 3 lots	\$100.00	\$150.00	\$150.00	
Per lot 1-4				\$225.00					
Per lot 5-49				\$150.00	\$50.00				
Per lot 50 and over				\$125.00					
Legal Notice						Actual Cost			
Sub/Re-Sub app processing & inspection fees	Inc. in app								
Base fee		\$100							
Erosion control fee		\$50 per lot							
Inspection fee		\$2/linear ft of new road							\$50 per lot & 5% of est. cost of infrasture construction
Per lot fee		\$100							
Road/utility document review fee		\$500							
Each Structure (catch basin,manhole cover etc					\$25 each				
Per Linear ft of new road or ext. of existing road					\$5	\$1			
Detention Basin					\$500				
Cul-de-sace Fee					\$2,500				
Legal notice						Actual cost			
Engeneering Review for storm drainage, utilities and new road									\$500 plus \$1/linear foot of road

