

Stacy Radford
Zoning & Wetland Enforcement Officer
Department of Land Use & Development
Town of Montville
310 Norwich–New London Turnpike
Uncasville, CT 06382

RE: Inland Wetlands Application 24 IWC 7
339 Chesterfield Road – Culvert Crossing
Response to Item 5 / Item 6

Dear Ms. Radford,

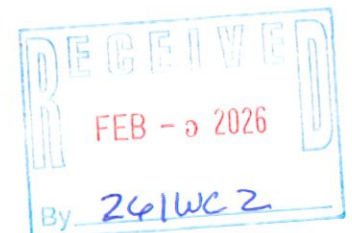
This letter is submitted in response to Item 5 and Item 6 of the CLA Engineers, Inc. review letter dated January 23, 2026, specifically regarding the elevation of the culverts at the driveway crossing.

The culverts as constructed are installed approximately six (6) inches higher than the originally approved elevation. Based on a review of site conditions and the hydraulic characteristics of the crossing, this minor vertical deviation does **not** impede hydraulic function, nor does it result in adverse wetland impacts.

The contributing drainage area to the crossing is very small, and observed flows are minimal under both normal and storm conditions. As constructed, the crossing does **not** overtop, does **not** cause upstream ponding or damming of flows, and does **not** present a risk of washout or downstream erosion. The existing channel remains continuous through the crossing, and conveyance capacity is more than sufficient for anticipated flows.

It is also important to note that the culverts were **intentionally oversized** relative to the contributing watershed. This sizing was not driven by hydraulic demand, but rather by environmental considerations, including the accommodation of wildlife passage and the minimization of ecological fragmentation. As a result, the available hydraulic capacity far exceeds the flow rates experienced at the site, rendering the minor elevation difference hydraulically insignificant.

Given the very low flow regime, the oversized culvert geometry, and the absence of any observed or anticipated hydraulic or erosional impacts, resetting the culverts to the originally approved elevation would not provide a measurable environmental or hydraulic benefit. Conversely, such work would introduce additional disturbance within the wetland area that is not warranted based on site performance.



Based on the above, it is our professional opinion that the culvert crossing, as constructed, functions properly, conforms to the intent of the applicable regulations, and does not adversely affect wetland resources.

Please feel free to contact me if you have any questions or would like to discuss this further.

Respectfully submitted,



Ryan E. Thompson PE, LS

RCL Thompson LLC

